

LETTER OF AGREEMENT

between

IVAO ATC HQ
Maastricht UAC

and

IVAO BeLux
Brussels ACC



Effective – 1 April, 2021

1 General

1.1 Purpose

The purpose of this Letter of Agreement is to define the coordination procedures to be applied between Maastricht UAC and Brussels ACC when providing ATS to General Air Traffic and Operational Air Traffic.

These procedures are supplementary to those specified in IVAO Documentation and/or Divisional website Documents.

1.2 Operational Status.

Both Divisions shall keep each other advised of any changes in the operational status of their facilities and navigational aids, which may affect the procedures specified in this Letter of Agreement.

2 Areas of Responsibility for the Provision of ATS

2.1 Areas of Responsibility

The lateral and vertical limits of the respective areas of responsibility are as follows:

Note: See para 2.2 for the description of the areas where delegation of the responsibility for the provision of ATS is applicable.

2.1.1 Maastricht UAC

Lateral limits: Brussels FIR/UIR as published in the AIP Belgium and G.D. Luxembourg

Vertical limits: FL 245 – UNL

ICAO airspace classification for the area of responsibility of Maastricht UAC along the common boundary of the areas of responsibility of Maastricht UAC and Brussels ACC is described in Annex B to this Letter of Agreement.

2.1.2 Brussels ACC

Lateral limits: Brussels FIR/UIR as published in the AIP Belgium and G.D. Luxembourg

Vertical limits: GND – FL 245

ICAO airspace classification for the area of responsibility of Brussels ACC along the common boundary of the areas of responsibility of Maastricht UAC and Brussels ACC is described in Annex B to this Letter of Agreement.

2.2 Areas for Cross Border Provision of ATS (ATS Delegation)

The provision of ATS in respect of this LoA means the following services:

Air Traffic Control Service (ATC), Flight Information Service (FIS) for controlled flights, Alerting Service (ALRS)

2.2.1 Delegation of ATS from Maastricht UAC to Brussels ACC

Not applicable.

2.2.2 Delegation of ATS from Brussels ACC to Maastricht UAC

Not applicable.

2.2.3 Other Areas for Cross Border Provision of ATS

Areas for cross-border provision of ATS defined with other coordinating air traffic services units along the common boundary of the areas of responsibility of Maastricht UAC and Brussels ACC are described in Annex B to this Letter of Agreement.

2.2.4 Alerting Service

The ATS unit responsible for the provision of ATS, by virtue of delegation, shall provide alerting service and shall notify immediately the supervisor of the delegating ATS unit. The supervisor of the delegating ATS unit shall notify the appropriate rescue coordination centre as required.

2.2.5 Territorial Matters

- Not applicable -

2.3 Special Provisions

- Not applicable -

3 Procedures

The procedures to be applied by Maastricht UAC and Brussels ACC are detailed in the Annexes to this Letter of Agreement:

Annex A	Definitions and Abbreviations
Annex B	Area of Common Interest
Annex C	Exchange of Flight Data
Annex D	Procedures for Coordination
Annex E	Transfer of Control and Transfer of Communications
Annex F	ATS Surveillance Based Coordination Procedures
Annex G	Checklist of Pages

4 Revisions and Deviations.

4.1 Revision of this Letter of Agreement

The revision of the present Letter of Agreement, excluding Annexes and their Appendices, requires the mutual written consent of the signatories.

4.2 Revision of the Annexes to the Letter of Agreement.

The revision of Annexes to the present Letter of Agreement requires the mutual written consent of the representatives of the respective IVAO Divisions designated by the respective signatories, normally the ATC Operations Coordinator at the respective Division.

4.3 Temporary Deviations.

When necessary, the ATC Department of the IVAO Divisions concerned may introduce, by mutual agreement and for a specified period of time, temporary modifications to the procedures laid down in the Annexes to the present Letter of Agreement.

4.4 Incidental Deviations.

Instances may arise where incidental deviations from the procedures specified in the Annexes to this Letter of Agreement may become necessary. Under these circumstances air traffic controllers are expected to exercise their best judgement to ensure the safety and efficiency of air traffic.

5 Cancellation.

5.1 Cancellation of the present Letter of Agreement by mutual written agreement of the respective IVAO Divisions may take place at any time.

5.2 Cancellation of this Letter of Agreement by either IVAO Division is possible at any time, provided that the cancelling party declares its intention in writing to cancel the Letter of Agreement with a minimum pre-notification time of 6 months before the date the cancellation is to take effect.

6 Interpretation and Settlement of Disputes.

- 6.1 Should any doubt or diverging views arise regarding the interpretation of any provision of the present Letter of Agreement or in case of dispute regarding its application, the parties shall endeavor to reach a solution acceptable to both of them.
- 6.2 Should no agreement be reached, each of the parties shall refer to IVAO HQ ATC Operations Department, to which the dispute shall be submitted for settlement.

7 Validity

This LoA becomes effective 1 April, 2021 and supersedes the Letter of Agreement between Maastricht ACC and Brussels ACC dated N/A.

Shaun Ellis – 471730
ATC Operations Director
IVAO ATC HQ

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ATC Ops Coordinator
BeLux

Annex A.

Definitions and Abbreviations

Effective: 2021-04-01

Revised: N/A

A.1 Definitions.

A.1.1 Area of Responsibility

An airspace of defined dimensions where a sole ATS unit has responsibility for providing air traffic services.

A.1.2 Area of Common Interest

A volume of airspace as agreed between two ATS units, extending into the adjacent/subjacent Areas of Responsibility, within which airspace structure and related activities may have an impact on air traffic coordination procedures.

A.1.3 Approval Request

Request from an ATS-unit to the ATS sector concerned for an approval of:

- an aircraft not yet airborne, whenever the flying time to the transfer of control point is less than the agreed minimum prenotification time, or
- an aircraft in flight intending to operate under conditions other than those described in mutually agreed procedures.

A.1.4 Division Level.

The flight level dividing two superimposed AoR for the provision of ATS.

A.1.5 General Air Traffic.

All flights which are conducted in accordance with the rules and procedures of ICAO and/or the national civil aviation regulations and legislation.

A.1.6 Operational Air Traffic.

All flights which do not comply with the provisions stated for GAT and for which rules and procedures have been specified by appropriate national authorities.

A.1.7 Reduced Vertical Separation Minimum.

A vertical separation minimum of 300 m (1 000 ft) which is applied between FL 290 and FL 410 inclusive, on the basis of regional air navigation agreements and in accordance with conditions specified therein.

A.1.7.1 RVSM Approved Aircraft

Aircraft that have received State approval for RVSM operations within the EUR RVSM airspace.

A.1.8 Release

A.1.8.1 Release for Climb

An authorization for the accepting sector to climb (a) specific aircraft before the transfer of control.

Note: The transferring sector remains responsible within its Area of Responsibility for separation between the transferred aircraft and other aircraft unknown to the accepting unit, unless otherwise agreed.

A.1.8.2 Release for Descent

An authorization for the accepting sector to descend (a) specific aircraft before the transfer of control.

Note: The transferring sector remains responsible within its Area of Responsibility for separation between the transferred aircraft and other aircraft unknown to the accepting unit, unless otherwise agreed.

A.1.8.3 Release for Turn

An authorization for the accepting sector to turn (a) specific aircraft away from the current flight path by not more than 45° before the transfer of control

Note: The transferring sector remains responsible within its Area of Responsibility for separation between the transferred aircraft and other aircraft unknown to the accepting unit, unless otherwise agreed.

A.1.9 State Aircraft

For the purposes of EUR RVSM, only aircraft used in military, customs or police services shall qualify as State aircraft.

A.2 Abbreviations.

ACC	Area Control Center	NM	Nautical Mile
ACI*	Area of Common Interest	NM	EUROCONTROL Network Management
AIP	Aeronautical Information Publication	OAT*	Operational Air Traffic
AoR*	Area of Responsibility	OLDI*	On-line Data Interchange
APP	Approach Area / Approach ATS Unit	ORCAM	Originating Region Code Assignment Method
ATC	Air Traffic Control	RTF	Radio Telephony
ATS	Air Traffic Services	RVSM	Reduced Vertical Separation Minimum
ATZ	Air Traffic Zone	SFC	Surface
CBA	Cross-Border Area	SID	Standard Instrument Departure
CDR	Conditional Route	SSR	Secondary Surveillance Radar
COP*	Coordination Point	STAR	Standard Instrument Arrival
CRC	Control and Reporting Centre	TMA	Terminal Maneuvering Area
CTA	Control Area	TRA	Temporary Reserved Area
CTR	Control / Enroute ATS Unit	TSA	Temporary Segregated Area
CTZ	Control Zone	TWR	Aerodrome Control Tower
DFL*	Division Flight Level	UAC	Upper Area Control Centre
ETO	Estimated Time Over Significant Point	UHF	Ultra High Frequency
EUR	European	UIR	Upper Flight Information Region
FIR	Flight Information Region	UNL	Unlimited
FLA*	Flight Level Allocation	UTC	Coordinated Universal Time
FIS	Flight Information Services	VCS	Voice Communication System
FL	Flight Level	VFR	Visual Flight Rules
GAT*	General Air Traffic	VHF	Very High Frequency
GND	Ground		
ICAO	International Civil Aviation Organization		
IFR	Instrument Flight Rules		
KHz	Kilohertz		
LoA*	Letter of Agreement		
MHz	Megahertz		

Note: Abbreviations marked with an * are non-ICAO abbreviations.

Annex B.

Area of Common Interest

Effective: 2021-04-01

Revised: N/A

B.1 Airspace Structure and Classification within the Area of Common Interest.

The Airspace structure within the ACI is shown in Appendix of Annex B

B.1.1 Brussels FIR/UIR

Area	Vertical Limits	Airspace Classification
Brussels UTA ¹⁾	FL 195 – FL 245	C
Brussels UTA ²⁾	FL 245 – FL 660	C

1) ATS provided by Brussels ACC

2) ATS provided by Maastricht UAC

B.2 Sectorisation within the Area of Common Interest

The Sectorisation within the ACI is shown in Appendix 1 of Annex B

B.3 Special Areas within the Area of Common Interest

B.3.1 Areas for Cross/Border Provision of ATS defined with other ATS Units within the ACI.

B.3.1.1 SASKI A Area – See Appendix 2 of Annex B

The provision of Air Traffic Services in that portion of the Amsterdam FIR, south of a parallel line 5 NM north of the ATS route L179/L608 is delegated to Brussels ACC.

Vertical limits: FL55 – FL245

Airspace Classification: C above FL195

A below FL195

B.3.1.2 Maastricht Area – See Appendix 2 of Annex B

Within the Amsterdam FIR the responsibility for the provision of ATS in accordance with the airspace classification has been delegated to Brussels ACC within the following area:

Lateral Limits: The part of the Amsterdam FIR linking the co-ordinates:

N511521 E0053324 – N511446 E0060454 – along the Dutch-German border to
N504516 E006011440 – along the Dutch-Belgian border to N511521 E0053324

Vertical limits: FL095 – FL245

Airspace Classification: C

B.3.1.3 BITBU Area – See Appendix 2 of Annex B

Maastricht UAC is responsible for the provision of Air Traffic Services to GAT within the following part of the Rhein UIR:

Lateral Limits: N494833 E0063010 – N495604 E0063632 – N501628 E0063903 – N502541
E0063751 then along the Hannover/Rhein UIR boundary till N502000 E0062430
then along the boundary line of the Brussels / Rhein UIR to N494833 E0063010.

Vertical limits: FL245 – UNL

Airspace Classification: C

B.3.1.4 NORVENICH Area – See Appendix 2 of Annex B

Maastricht UAC is responsible for the provision of Air Traffic Services within the following part of the Hannover UIR:

Lateral Limits: N510403 E0055739 – N505008 E0065857 – N503550 E0070802 and along the
Hannover UIR/Rhein UIR boundary, the Belgian/German and Dutch/German
boundary to N510403 E0055739.

Vertical limits: FL245 – UNL

Airspace Classification: C

B.3.1.5 PINOT - SORAL Area – See Appendix 2 of Annex B

The provision of Air Traffic Services is delegated from Reims UAC to Maastricht UAC within the following area

Lateral Limits: North of line defined by N484903 E0061636 – N491350 E0094109

Vertical limits: FL245 – UNL

Airspace Classification: C

B.3.2 Other Areas

Within the BULAM area, the use FL250 is permanently attributed to Brussels ACC. Maastricht UAC shall not assign FL250 in this area without prior co-ordination with Brussels ACC.

BULAM area - see Appendix 7.

Lateral limits: Straight lines joining the following positions

1. N512402 E0020000
2. N513000 E0020000
3. N512720 E0023000
4. N513813 E0023000
5. N512537 E0032423
6. N511334 E0032423
7. N512402 E0020000

Vertical limits: FL245 to FL255

Airspace classification: C

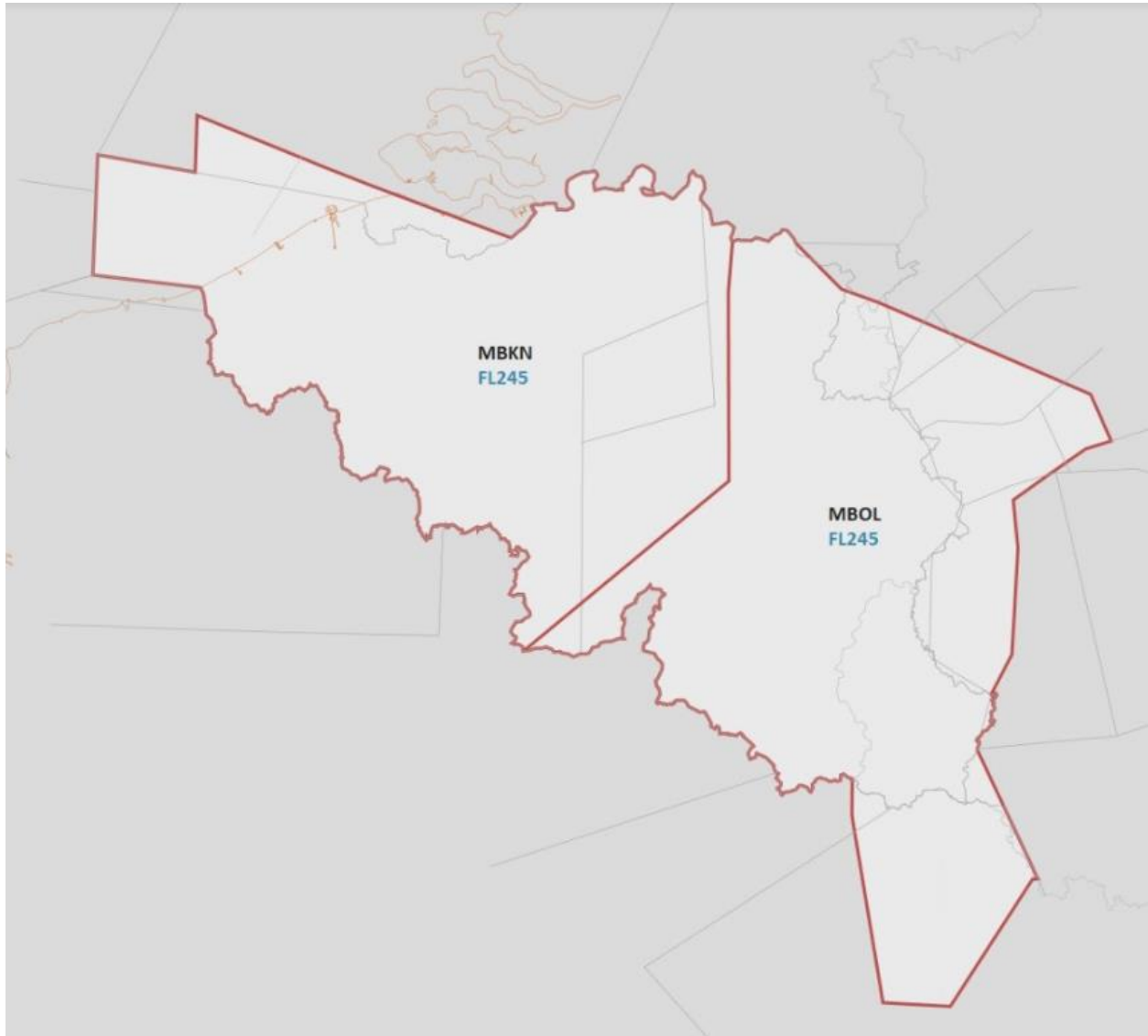
B.4 Non-published Coordination Points within the Area of Common Interest.

QPITE N503517 E0044502

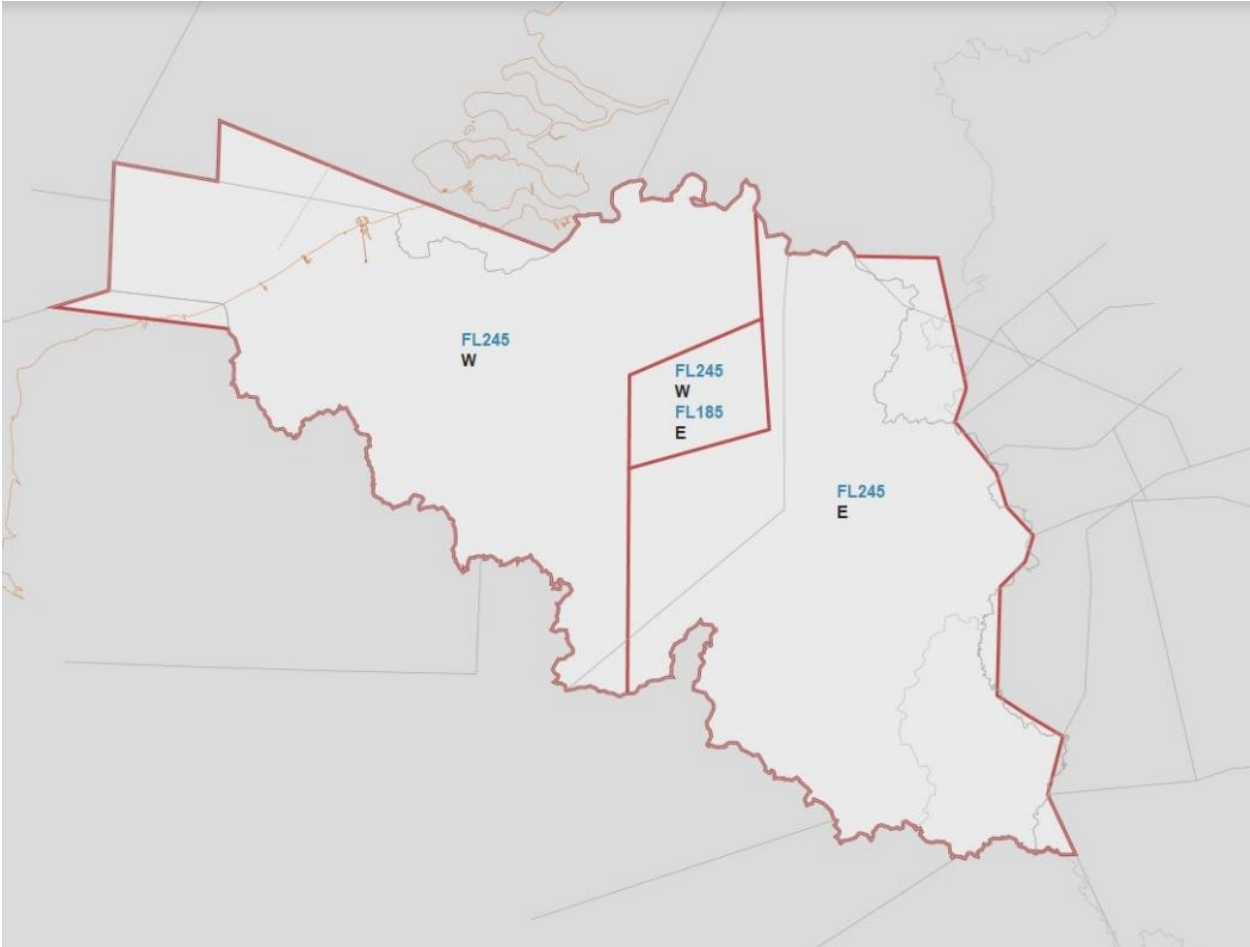
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Appendix 1 of Annex B.

Maastricht UAC Sectorization

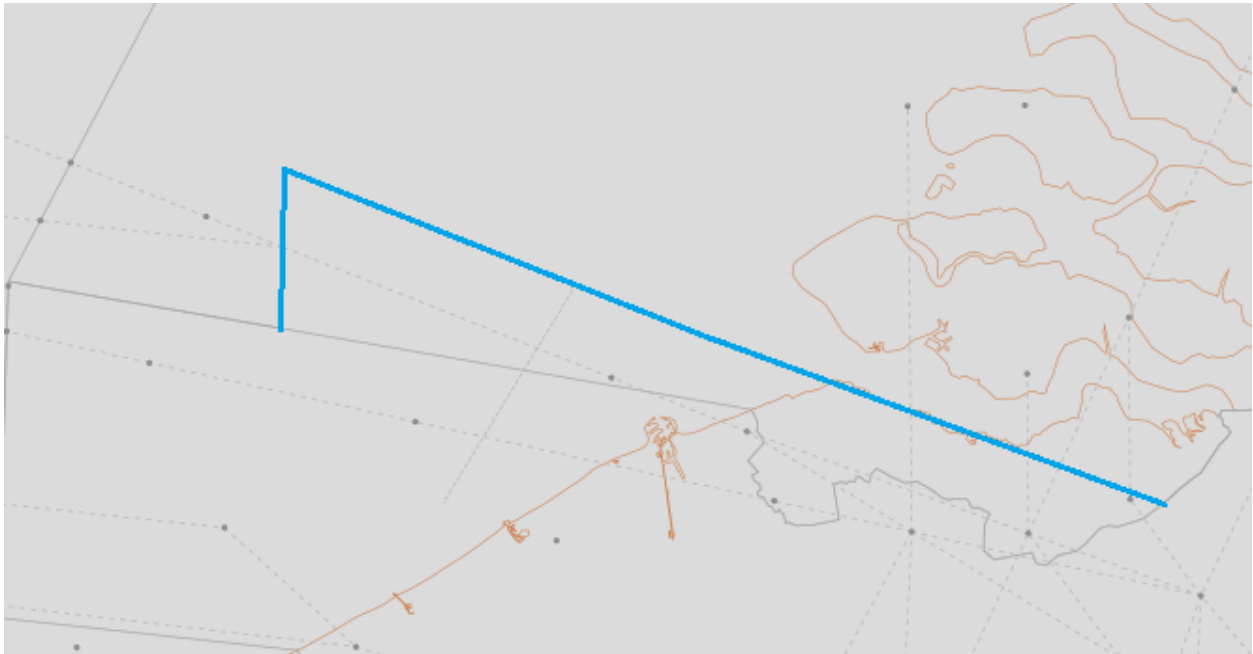


Brussels ACC Sectorization

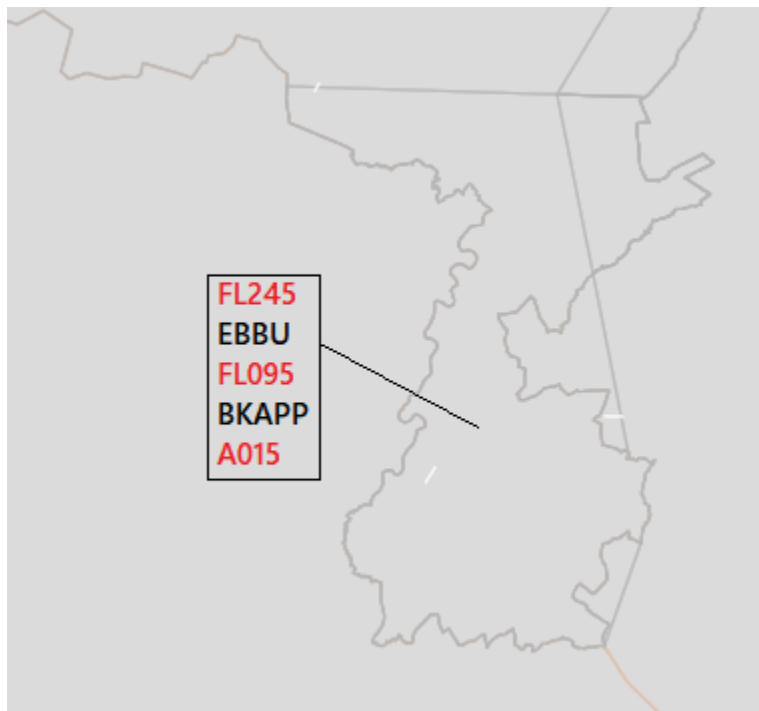


Appendix 2 of Annex B.

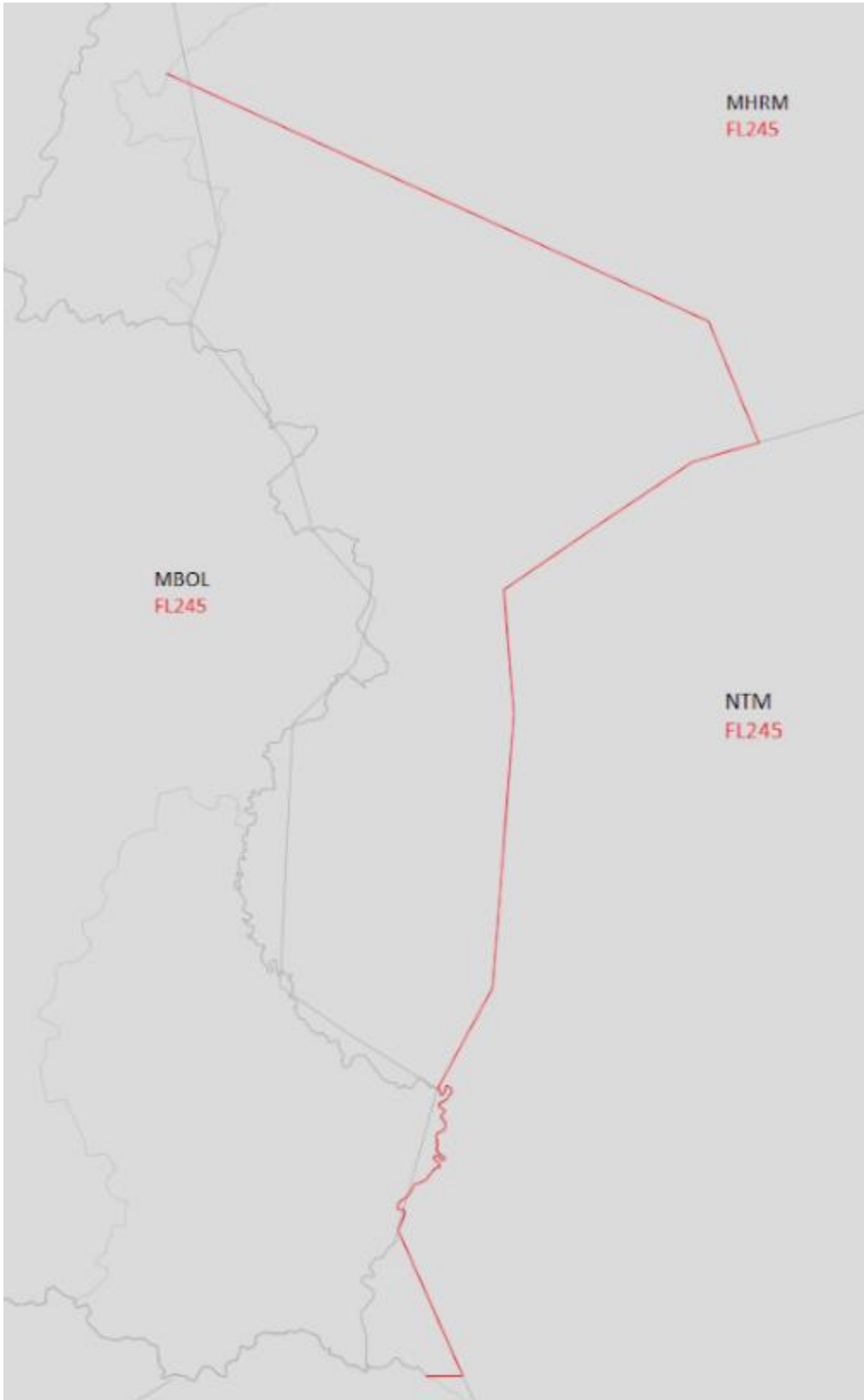
SASKI A AREA



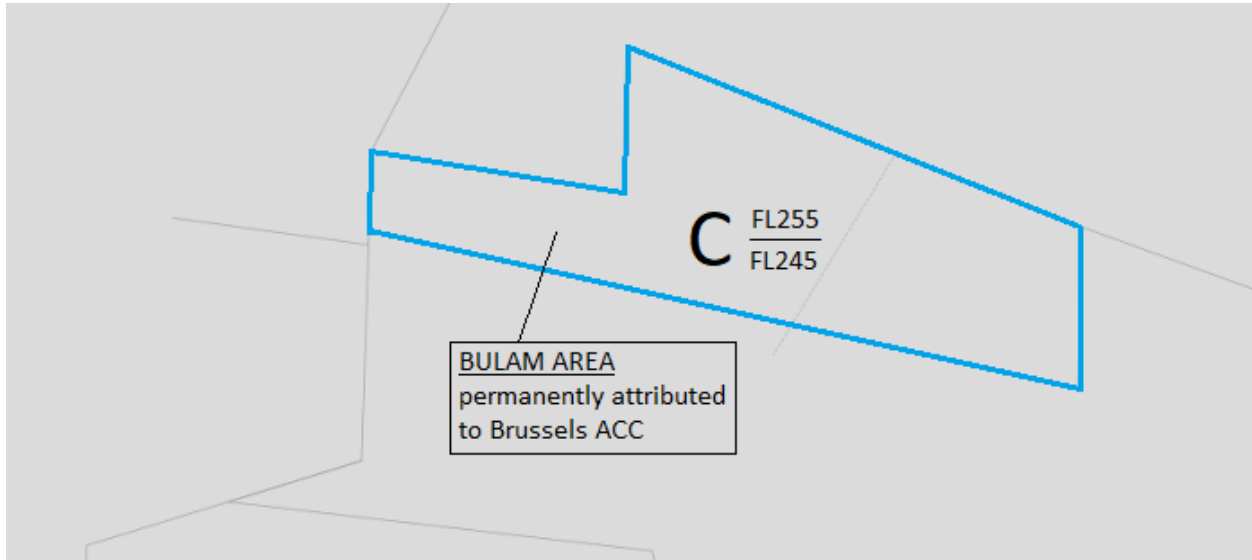
MAASTRICHT AREA



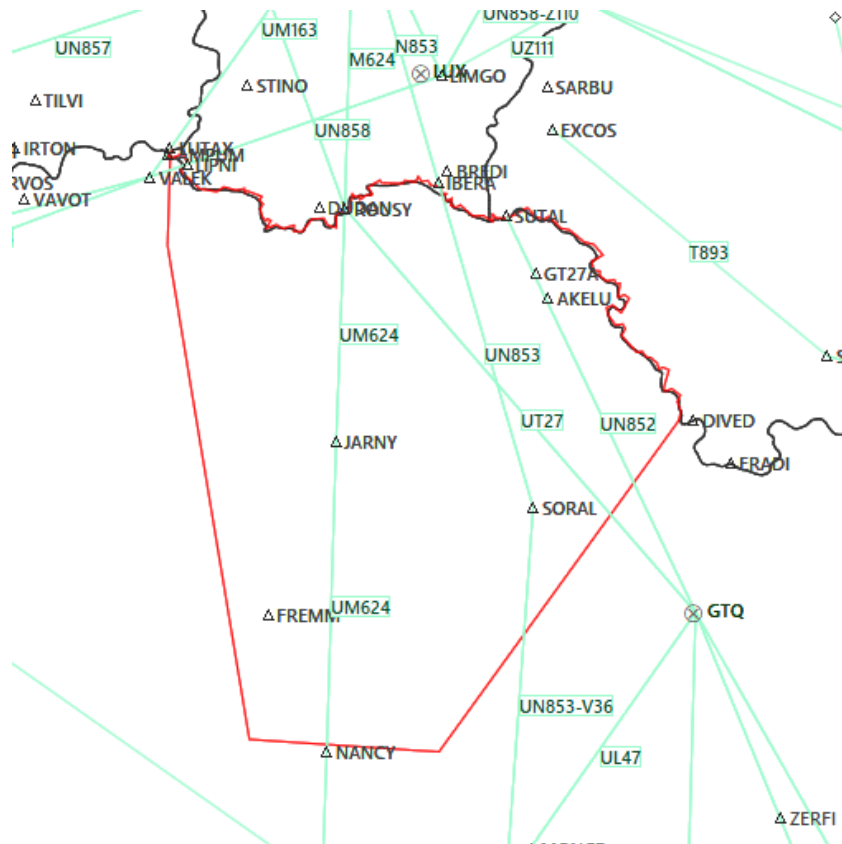
BITBU - NORVENICH AREA



BULAM AREA



PINOT - SORAL AREA



Annex C.

Exchange of Flight Data

Effective: 2021-04-01

Revised: N/A

C.1 General

C.1.1 Basic Flight Plans

Basic Flight plan data should normally be available at both ATS Units.

C.1.2 Current Flight Plan Data

Messages, including current flight plan data, shall be forwarded by the transferring ATS unit to the accepting ATS unit either by automatic data exchange or by private text to the appropriate sector/position.

C.1.3 Revisions

Any significant revisions to the flight data are to be transmitted to the accepting ATS Unit.

Changes to the coordinated levels within 5 minutes of the ETO for the transfer of control point are subject to an Approval Request.

C.2 Means of Communications and their Use

C.2.1 Verbal Coordination

Not applicable due to current software limitations.

C.2.2 Written Communication

When required, communication can be performed by private text between the relevant sectors or using semi-automatic systems depending on software capability.

Annex D.

Procedures for Coordination

Effective: 2021-04-01

Revised: N/A

D.1 General Conditions for Acceptance of Flights

- D.1.1 Coordination of flights shall take place by reference to the COP for the relevant route and in accordance with the appropriate flight levels specified for the relevant route (see para D.2 and D.3).
- D.1.2 Flights shall be considered to be maintaining the coordinated level at the transfer of control point unless climb or descent conditions have been clearly stated by use of written coordination, except if otherwise described in paragraphs D.2 or D.3.
- D.1.3 If the accepting ATS Unit cannot accept a flight offered in accordance with the conditions specified above it shall clearly indicate its inability and specify the conditions under which the flight will be accepted.
- D.1.4 For any proposed deviation from the conditions specified in this Annex (e.g. COP, route or level) the transferring Unit shall initiate an Approval Request.
- D.1.5 The accepting ATS Unit shall not notify the transferring ATS Unit that it has established ground-air communications with the transferred aircraft unless specifically requested to do so. The accepting Unit shall notify the transferring Unit in the event that communication with the aircraft is not established as expected.

D.2 ATS Routes, Coordination Points and Flight Level Allocation

Available (ATS) routes, COPs to be used and flight allocation to be applied, unless otherwise described in paragraph D.3, are described in the tables below.

D.2.1 Flights from Maastricht UAC to Brussels ACC

Destination	Routing	COP	Cross allocated FL prior	Receiving Sector	FLA	Special Conditions
EBBR/EBMB, EBCI, EBAW, EBCV	DIK – BATTY	BATTY	20 NM south-east BATTY		FL250	
	ROUSY – BATTY	BATTY	20 NM south-east BATTY		FL250	See note 5
	LIPNI - FLO	LIPNI	Abeam SOPOK		FL250	Only DEST EBBR/MB See note 4
	LIRSU – BATTY	BATTY	10 NM prior BATTY		FL250	Only DEST EBBR/MB See note 6
	LAMLA - BATTY	BATTY	10 NM prior BATTY		FL250	Only DEST EBBR/MB See note 6
EBOS	ADKUV/NVO - SOGRI	SOGRI	SOGRI		R/R	
	IBERA/ROUSY-REMBA	REMBA	REMBA		R/R	
ELLX, LFSF, EDDR, EDRZ	KOK/TOLEN/TULIP /LARAS/GISEB – REMBA – SPI	REMBA	REMBA		FL250	See note 2
	KOK/TOLEN/TULIP /LARAS/GISEB – REMBA – RITAX	REMBA	REMBA		R/R	
	KOK - KOMOB	KOMOB	Abeam REMBA		R/R	
	VELED - LNO	LNO	LNO		R/R	
EDFH, EDRK	KOK/TOLEN/TULIP /LARAS/GISEB – REMBA	REMBA	REMBA		FL250	See note 2
EBZW, EHBD, EHGR, EHVK	IBERA/ROUSY - DIK	DIK	DIK		R/R	
EHEH	ADUTO - FERDI	FERDI	5 NM North of UL607		FL250	
	IBERA/ROUSY - DIK	DIK	DIK		R/R	

LFST/JL/GA/ SG/SN/SO	KOK/TOLEN/TULIP /LARAS – RITAX – DIK	DIK	AXIS OF LNO - RASCA		R/R	
EDJA/SB/TD/ TF/TL/ EDDS, LSZR	LARAS/VELED/NV O – GESLO – DIK	DIK	GESLO		R/R	
LFST/JL/GA/ SG/SN/SO, LSZR, EDJA/TD/TF/ TL	KOK – DIK	DIK	AXIS OF LNO – RASCA		R/R	
EDDK, EDGS, EDKB, EDKL, EDLM, EDLE	KOK – REMBA	REMBA	REMBA		FL250	See note 2
	IBERA/ROUSY – DIK	DIK	DIK		R/R	
EDDK, EDGS, EDKB, EDKL	ARDEN – KOMOB	KOMOB	KOMOB		R/R	See note 1
EDLV, EDLN	KOK – REMBA	REMBA	REMBA		FL250	See note 2
EHAM	BELOB – DENUT	DENUT	5 NM North of UL607		FL250	See note 3
	ADUTO – FERDI – DENUT	DENUT	5 NM North of UL607		FL250	
	ROUSY/IBERA/LIRS U/ADKUB – BUB	BUB	BUB		FL250	
EHRD, EHLE	ADUTO – FERDI	FERDI	5 NM North of UL607		FL250	
	REMBA/SOGRI – BUB	BUB	BUB		FL250	
LFOI, LFOH, LFRG, LFOB, LFOP, LFOE, LFOK, LFRC, LFRK	IBERA/ROUSY – REMBA	REMBA	REMBA		R/R	
	ADKUV/NVO – SOGRI	SOGRI	SOGRI		R/R	
	NIK – CIV	CIV	UL607		R/R	
EGMD, LFQT, LFQQ, LFQO, LFAV, LFAC, LFAT, LFAQ, LFAY	IBERA/ROUSY – REMBA	REMBA	REMBA		R/R	
	ADKUV/NVO – SOGRI	SOGRI	SOGRI		R/R	

Notes:

- 1) When weekend routings are available.
- 2) Brussels ACC shall ensure separation with EBBR TMA arrivals at FL250 on route segment DIK BATTY.
- 3) Within the Brussels UIR, Brussels ACC may consider traffic on UY873 to be released to FL200.
- 4) Only available between 23:00 – 05:00 UTC wintertime.
- 5) Maastricht UAC shall ensure separation with EBBR/EBMB outbounds on ROUSY departures.
- 6) Night procedure available between 00:00 and 05:00 UTC wintertime

D.2.2 Flights from Brussels ACC to Maastricht UAC

Origin	Routing	COP	Cross allocated FL prior	Receiving Sector	FLA	Special Conditions
EBBR/EBMB	SOPOK - ETENO	BULUX	FIR/UIR boundary	LNO	FL240	See note 2 & 5
	REMBA - (BULUX) – RITAX - ROUSY	REMBA	abeam DIK	LUX	FL240	See note 1
	ROUSY night SID	ROUSY				-
	REMBA - (BULUX) – RITAX – DIK - PITES	REMBA	DIK	LUX	FL240	See note 1, 7
	PITES night SID	QPITE				-
EBCI/CV	SOPOK - ETENO	BULUX	FIR/UIR boundary	LNO	FL240	See note 2 & 5
	REMBA - (BULUX) – RITAX – ROUSY/PITES	REMBA	abeam DIK	LUX	FL240	See note 1
EBCI	CIV – COA – UL745 CIV – FERDI – HELEN	FERDI	5 NM south UL610	KOK	R/R	see note 8
EBAW	REMBA – SPI – MATUG/LIRSU	REMBA	SPI	LNO	FL240	See note 6
	REMBA – RITAX	REMBA	abeam DIK	LUX	FL240	See note 1
EBBL		LNO		LNO	R/R	
EHEH	LNO – SOPOK – ETENO	LNO	FIR/UIR boundary	LNO	FL240	See note 1 & 5
	LNO – SOPOK – RITAX	LNO	abeam DIK	LNO	R/R	
	LNO – RASCA	LNO	-	LNO	R/R	
Langen FIR Except EDDF, EDFE, EDFM, EDDS, EDFH, EDRZ	NAVAK – LNO – SOPOK	LNO	15 NM South LNO	LNO	FL240	
	NAVAK – SOGRI	SOGRI	SOGRI	LNO	FL240	
	NAVAK – NIK	NAVAK	abeam SOGRI	LNO	FL240	Only DEP EDDK
	NAVAK – LNO – RASCA	NAVAK	15 NM South West LNO	LNO	FL240	
	MODRU – MAS – SOGRI	SOGRI	SOGRI	LNO	FL240	Only DEP EDDL, EDLV
	NETEX – RASCA	NETEX	Y868	LNO	FL240	Only DEP EDDL, EDLV See note 1
	NETEX – DIBIR – NIK	NETEX	5NM West of N852	LNO	R/R	See note 9
	NVO - SOPOK	SOPOK	SOPOK	LNO	FL240	Only DEP EDDK

EDDF, EDFE, EDFH, EDFZ	BITBU-DEMUL	DEMUL	-	LUX	R/R	
EDDR, EDFM, EDR	ROBON-RITAX	RITAX		LUX	R/R	
Amsterdam FIR	WOODY-NIK-CIV	NIK	20 NM North CIV	NIK	FL240	See note 3
	WOODY-NIK-BUB	NIK	BUB	NIK	FL240	See note 3
	BROGY-LNO	BROGY	Y868	LNO	FL240	See note 1, 4
London TMA	KONAN - KOK	KOK	-	KOK	R/R	
EBOS, EBKT, EBFN, LFAC, LFAQ, LFAT, LFAV, LFQO, LFQQ, LFQT	REMBA	REMBA	-	NIK	R/R	
	KOMOB	KOMOB	-	NIK	R/R	
EBLG, EHBK	SOGRI-BUB	BUB	-	NIK	R/R	
	LNO-GESLO	GESLO	-	LUX	R/R	
LFAV	FERDI	FERDI	5 NM south L610	KOK	R/R	see note 8
Paris TMA	ADUTO - FERDI	FERDI	-	KOK	R/R	
LFST	SUTAL - DIK	DIK	-	LUX	R/R	
LFJL, LFSF	DIK - LNO	LNO	-	LNO	R/R	
	DIK - REMBA	REMBA	-	LUX	R/R	
ELLX	DIK - LNO	LNO	-	LNO	R/R	

Notes:

- 1) Traffic unable to meet this climb requirement is subject to a radar to radar co-ordination to agree on a level above the DL. Brussels ACC shall ensure internal co-ordinations, when appropriate, before transferring traffic.
- 2) Flights unable to cross the UIR boundary FL250 or above shall be coordinated between Brussels ACC and Langen ACC at a level below FL245.
- 3) A level above FL245, given by Maastricht UAC, will apply south of radial 090° HSD.
- 4) A level above FL245, given by Maastricht UAC, will apply south of EHN. Brussels ACC is responsible for separation between such flights and Langen FIR departures towards SOGRI.
- 5) For flights unable to cross 5 NM east SOPOK (= crossing N852) FL240 or above, Brussels ACC shall initiate a climb co-ordination with Langen ACC. For flights able to cross 5 NM east SOPOK FL240 or above Maastricht UAC will if necessary, initiate a climb co-ordination with Langen ACC.
- 6) Traffic unable to meet this restriction (SPI at FL240+) will be tactically radar vectored by Brussels ACC via SPI - LIRSU and is subject to radar to radar coordination to agree on a level above the DFL. Maastricht UAC is responsible for eventual climb coordination with Langen ACC.
- 7) For flights unable to cross DIK FL240, Brussels ACC shall initiate a climb co-ordination with Langen ACC. For flights able to cross DIK FL240 or above Maastricht UAC will, if necessary, initiate a climb coordination with Langen ACC.
- 8) Not available for traffic via L608, L610, L179. Flights unable to cross the DL at or before 5NM South of the L610 shall be coordinated between Brussels ACC and Amsterdam ACC at a level below FL245.
- 9) Langen ACC and Brussels ACC shall ensure that this traffic is not vectored North of ATS route L179 centerline.

D.3 Special Procedures

D.3.1 Flights from Maastricht UAC Brussels sectors to Brussels ACC

D.3.1.1 The following flights shall enter the Brussels UIR below FL245

- EBBR/EBMB, EBAW, EBCV and EBCI arrivals entering the Brussels FIR/UIR via TOBIV, NIK, KOK, LENDO or KEVIN
- Arrivals EHBK, EBLG
- Arrivals ELLX except if routed via KOK, NIK or BROGY
- Arrivals EBOS except if routed via BATTY or REMBA
- Arrivals EDLN, EDFH routed ROUSY/IBERA-DIK
- Arrivals EDDR, EDFH, EDLN, EDLE, EHVK routed via MEDOX-DIK or ARDEN-KOMOB
- Arrivals EHEH if routed via DVR – KOK
- Arrivals LFQT, LFQQ, LFQO, LFAV, LFAT, LFAC, LFAQ expect if routed via DIK-REMBA or BATTY-SOGR

D.3.1.2 Traffic inbound to the Brussels FIR shall be descended by Maastricht UAC so as to pass the DFL at approximately 70NM from the destination, or as otherwise specified in D.2.1

D.3.1.3 Arrivals Routing via the NORVENICH area:

Traffic inbound EDK*, EDL* (except EDLN/LV), EDDL, EDDG and EDVK entering the Brussels UIR above the DL shall be coordinated directly between Maastricht UAC and Langen ACC.

Traffic with destination EDLV entering the Brussels UIR above the DFL shall be coordinated directly between Maastricht UAC and Langen ACC except if route via REMBA-UL607.

Note: In order to reduce the release requests in the LNO-NORVENICH area, Brussels ACC/Maastricht UAC agreed on a common release line after which the traffic is released to Langen ACC, defined by the following points:

1. N505442 E0060343
2. N505140 E0060441
3. N505048 E0060427
4. N504515 E0060116
5. N504500 E0060200
6. N503611 E0061250
7. N502956 E0061544
8. N502445 E0062303

D.3.1.4 Sequencing of traffic

As far as possible Maastricht UAC shall establish longitudinal spacing between successive arriving aircraft before transfer is effected, in order to avoid an undue accumulation of traffic.

D.3.1.5 Frequency Allocation / coordination sector

Unless otherwise specified, traffic shall be coordinated with and transferred to the Brussels ACC sector at which the flight is expected to cross the DFL.

D.3.1.6 Arrivals via FERDI/DENUT

Brussels ACC will accept traffic separated on radar headings without prior coordination, considering the following conditions are met:

- Aircraft not vertically separated are laterally separated on headings, with a minimum of 6NM constant or increasing
- Aircraft are given a rate of descent or a descent restriction to meet the constraints listed in D.2.1
- Pilots are instructed to report the assigned headings, rate of descent or descent restriction to Brussels ACC on contact
- Aircraft shall remain inside the corridor limited on the West by the line GIRVI-LUMEN and on the East by the line ADUTO-HELEN

D.3.2 Flights from Brussels ACC to Maastricht UAC Brussels sectors

D.3.2.1 The following flights are not accepted in the Brussels UIR above FL245

- Westbound traffic via WOODY – NIK – COA or WOODY – NIK – KOK.
- Traffic entering the Brussels FIR/UIR below FL245 with destination in the Paris TMA, except traffic departing EHAM.
- Departures from the Brussels FIR, Langen FIR, EHEH, EHRD, EHBK with destination ELLX, LFJL, LFSF, LFST, EDDR, EDFM, EDFB and EDRZ.
- EBBR/EBMB, EBAW, EBCV, EBCI departures leaving the Brussels FIR/UIR via SPI/LNO – KENUM - ABAMI.
- EBBR/EBMB, EBAW, EBCV, EBCI departures leaving the Brussels FIR/UIR via ETENO with aircraft type B462.
- EBBR/EBMB, EBAW, EBCV, EBCI departures with destination LFSG
- Departures from EBLG except if routed via COA or GESLO.
- Departures from EHBK and ETNG routed via LNO – NTM or CIV – MEDIL.
- Departures ELLX, except if routed via LNO – BUB, LNO – BROGY, DIK – BUB.
- Departures LFJL, LFSF if routed via LNO – KENUM or ARCKY – KENUM.
- Departures LFOB, LFQQ except if routed via REMBA or DIK.
- Departures EDFH except if routed via REMBA.

D.3.2.2 Maastricht UAC assures a level above the DFL for these departures where it has been agreed that the DFL will be passed. Coordination to obtain a level above the DFL has to be initiated by Brussels ACC.

D.3.2.3 Direct transfer between Brussels ACC and London ACC

D.3.2.3.1 L179/L608

Brussels TMA and EBCI departure via COA – L179/L608 shall be coordinated directly between Brussels ACC and London ACC.

D.3.2.3.2 L610

FL250 is permanently released to Brussels ACC for departures from Brussels TMA and EBCI within the BULAM Area.

This traffic flow is released for climb to LACC after passing the Transfer of Control Point, and on condition that the traffic remains on/north of the L610 until crossing DIBLI.

D.3.2.4 Sequencing of traffic

As far as possible Brussels ACC shall establish longitudinal spacing between successive departing traffic before transfer is effected in order to avoid an undue accumulation of traffic.

D.3.2.5 Frequency allocation / coordination sector

Unless otherwise specified, traffic shall be coordinated with and transferred to the Maastricht UAC sector as specified in paragraph D2.2..

D.3.2.6 Direct Routeings

D.3.2.6.1 WOODY direct REMBA

Brussels ACC is allowed to clear EHAM and EHRD departures requesting a level above FL245 and planned via WOODY-NIK-BUB-REMBA from WOODY direct to REMBA without verbal coordination.

Traffic shall be climbed to FL240 maximum and Brussels ACC is responsible for separation between WOODY-REMBA traffic and all traffic planned to cross the DFL via SOGRI.

D.4 VFR Flights

Not applicable.

Annex E.

Transfer of Control and Transfer of Communications

Effective: 2021-04-01

Revised: N/A

E.1 Transfer of Control

E.1.1 Transfer of control shall take place at the AoR boundary, unless otherwise specified in paragraph E.3.

E.2 Transfer of Communication

E.2.1 Transfer of communication shall take place not later than the transfer of control, unless otherwise coordinated.

Transfer of CPDLC shall commence concurrently with transfer of voice communications.

E.2.2 Frequency allocation and sector arrangement of Maastricht UAC

E.2.2.1 Maastricht UAC Brussels Sectors

Receiving Sector	Vertical Limits	VHF
BKN	FL 245 – FL660	132.755 MHz
BOL	FL 245 – FL660	125.980 MHz

E.2.3 Frequency allocation and sector arrangement of Brussels ACC

E.2.3.1 Brussels ACC Sectors

Receiving Sector	Vertical Limits	VHF
W	GND – FL245	131.100 MHz
E	GND – FL245	128.200 Mhz

E.3 Specific Points for Transfer of Control and Transfer of Communications

Not applicable.

Annex F.

ATS Surveillance Based Coordination Procedures

Effective: 2021-04-01

Revised: N/A

F.1 General

- F.1.1 Transfer of identification and transfer of control between Maastricht UAC and Brussels ACC will be subject to the serviceability of the respective surveillance systems.
- F.1.2 In case of any doubt about the identity of an aircraft, nothing in the provisions of this Annex, prevents the use of other methods for the identification of an aircraft.

F.2 Transfer of Aircraft Identification

- F.2.1 Transfer of aircraft identification between Maastricht UAC and Brussels ACC is normally performed by:
- notification of A1000, indicating that the Mode S aircraft identification feature transmitted by the transponder has been verified; or
 - if the aircraft identification is not correct or has not been verified, or if the aircraft is not Mode S equipped: by notification of the aircraft discrete SSR code.
- F.2.2 When discrete SSR codes are used for transfer of identification, they shall be assigned in accordance with ORCAM.
- F.2.3 Any change of SSR code by the accepting ATS Unit may only take place after the transfer of control point.
- F.2.4 The accepting ATS Unit shall be notified of any observed irregularity in the operation of SSR transponders or ADS-B transmitters.
- F.2.5 In the event that the accepting ATS unit is unable to process code A1000, it shall immediately advise the transferring ATS unit. Thereafter, unless otherwise coordinated, the transferring ATS unit shall change relevant instances of A1000 to a discrete SSR code determined in accordance with ORCAM.

F.3 Transfer of Radar Control

- F.3.1 Radar Separation minimum shall be **5 NM**.

F.3.2 Transfer of Control without systematic use of direct communication (Silent Transfer of Control)

Transfer of control may be effected without systematic use of bi-directional speech facilities provided the minimum distance between successive aircraft about to be transferred is **10 NM** and constant or increasing.

F.3.2.1 The transferring controller shall inform the accepting controller of any level, speed or vectoring instructions given to aircraft prior to its transfer and which modify its anticipated flight progress at the point of transfer.

Note: When using Mach-number speed control, pilots concerned shall be instructed to report their assigned mach-number to the accepting ATS Unit upon initial contact.

F.3.2.2 The accepting controller may terminate the silent transfer of control at any time, normally with an advance notice of **10 minutes**.

F.3.3 Transfer of Control with use of direct communication

Transfer of control may be effected with the use of bi-directional speech facilities, provided the minimum distance between the aircraft does not reduce to less than **5 NM**, and:

- identification has been transferred to or has been established directly by the accepting controller;
- the accepting controller is informed of any level, speed or vectoring instructions applicable to the aircraft at the point of transfer;
- communication with the aircraft is retained by the transferring controller until the accepting controller has agreed to assume responsibility for providing ATS surveillance service to the aircraft. Thereafter, the aircraft should be instructed to change over to the appropriate frequency and from that point is the responsibility of the accepting controller.

Annex G.

Checklist of Pages

Effective: 2021-04-01

Revised: N/A

Part of LoA	Page	Date
LoA		2021-04-01
Appendix 1		2021-04-01
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Annex A	A1	N/A
Annex B	B1	N/A
Annex C	C1	N/A
Annex D	D1	N/A
Annex E	E1	N/A
Annex F	F1	N/A
Annex G	G1	N/A